

General Design Intent

• The design intent is outlined by the Madigan Estate Design Guidelines and Karratha Vernacular Design Principles. All development must give due regard to the requirements and intent of these documents.

Building Envelope

- Height as per Category B standards of R-Codes (2 storey).
- Overshadowing No restrictions.

Garage/ Outbuildings

- Maximum garage width is 7.0m (double door) including supporting structures.
- Freestanding sheds and storerooms must be constructed of the same or complementary materials, finishes, colours and roof style types as the main house to the satisfaction of the Shire of Roebourne and not be located within the front setback.
- Any garage or boat storage carport constructed within the nominated breezeway shall be an open carport. An enclosed garage may not be constructed within the breezeway.
- Garage / carport may have a nil setback to the side boundary where appropriately fire rated.
- · Garages for corner lots shall be located on the secondary street or laneway at the rear of the dwelling.
- · Carports and garages shall be set back 4.5m from the front boundary to allow for additional car parking within the front setback.

- Dwelling to address the primary street by way of design, fenestration, entry and must contain major opening(s).
- · Front doors of dwellings shall be clearly visible and recognisable from the front of the dwelling.
- · A high level of articulation and architectural interest in the built form, such as the use of balconies, shading devices, clear front entry ways, mix of textures/ materials and window treatments, is required to the front facade.

Fencina

• Fences to Road Reserve/ Drainage shall be no higher than 1.5 metres and shall be constructed of a tubular (steel or aluminium) type fencing and black in colour. No solid features or appendages shall be added to these fences due to wind loading requirements.

Private Open Space

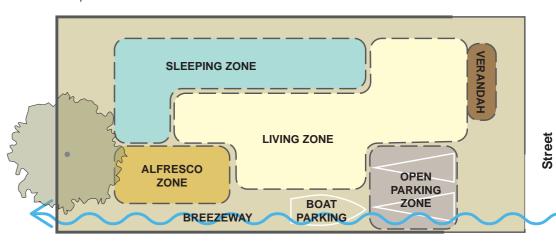
- There is no minimum open space requirement applicable to these lots if the setbacks and courtyard requirements are achieved, otherwise the provisions of the R-Codes apply.
- An alternative courtyard location may be approved for corner lots or those abutting a laneway provided appropriate solar protection is provided to prevent excessive heat gain to the interior of the dwelling and the courtyard and the cross ventilation requirements of the Madigan Road Design Guidelines are adequately demonstrated.

Servicing

- · Air conditioners or cooling units must not be easily visible from the street and be positioned to minimise noise impacts on neighbouring residences.
- Tanks associated with solar hot water units must either be ground mounted or match the roof profile and pitch and, where possible, not be easily seen from the street.
- Minor variations to the requirements of this Detailed Area Plan may be approved by the Shire of Roebourne without the need to obtain a separate planning approval. Significant variations will require a planning approval pursuant to the Scheme.

Indicative Floor Plan

This plan provides an example indicative floor plan to assist with interpreting the intent and principles outlined within the Detailed Area Plan. It does not form part of this Detailed Area Plan.



Context Plan - Lot Type A



Endorsed by:

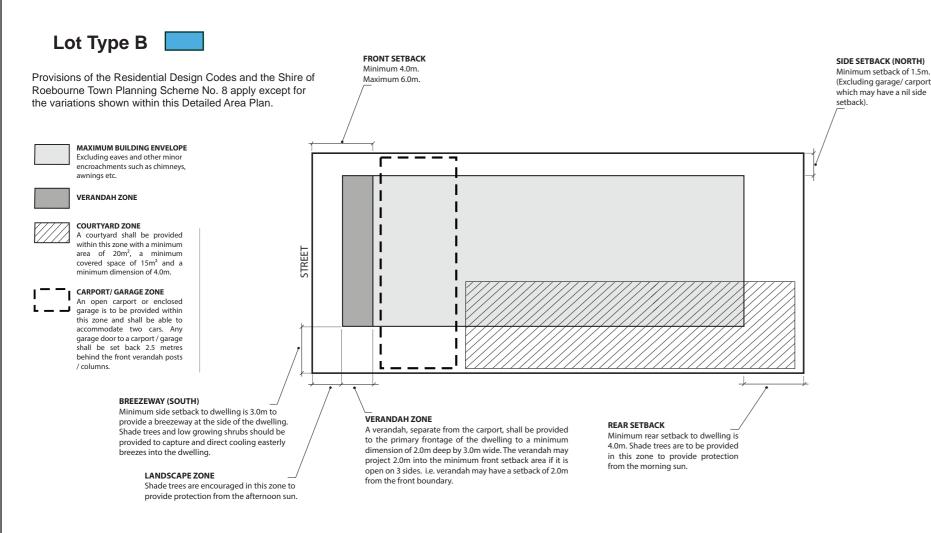
City of Karratha

Karratha, Madigan Road LOCAL DEVELOPMENT PLAN

Date: 16 Apr 2020

21/4/2020





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Building Frontage

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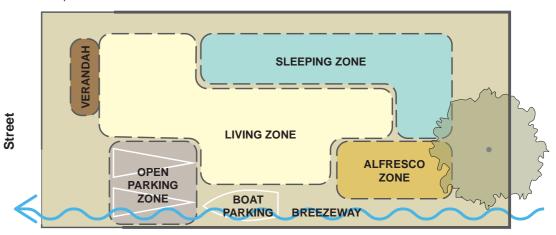
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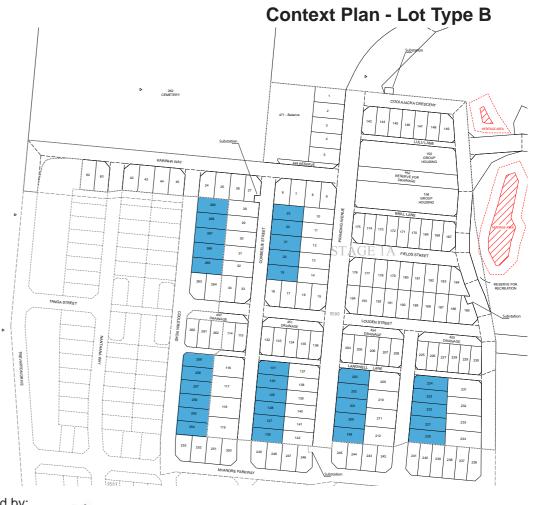
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Karratha, Madigan Road LOCAL DEVELOPMENT PLAN

Scale: N.T.S

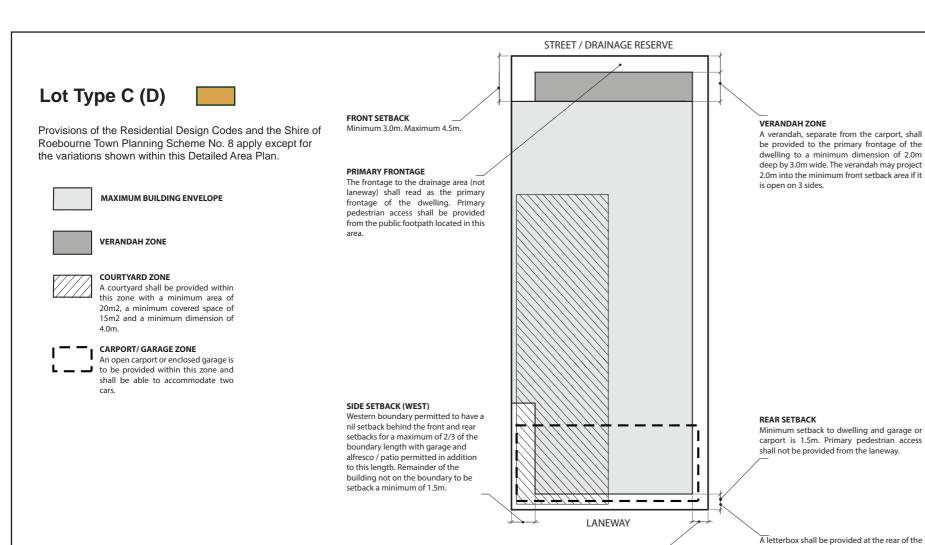
Drawing No. 710-200A Karratha, Madigan Road.indd



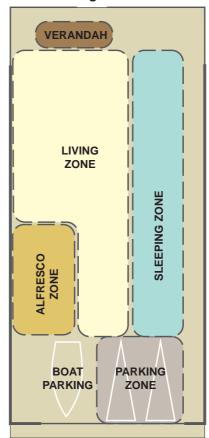
Level 18, 191 St Georges Terrace Perth Western Australia 6000

Date: 16 Apr 2020

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Drainage Reserve



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Context Plan - Lot Type C (D)

Lane

dwelling and setback 1.5m from the rear

City of Karratha

Provisions

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SIDE SETBACK (EAST)

Building Envelope

- · Height as per Category B standards of R-Codes (2 storey).
- Overshadowing No restrictions.

Garage/ Outbuildings

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- Garage / carport may have a nil setback to the side boundary where appropriately fire rated.
- No back gates are permitted where they encroach into the laneway when opening.

Building Frontage

- Dwelling to address the drainage reserve by way of design, fenestration, entry and must contain major opening(s).
- Front doors of dwellings shall be clearly visible and recognisable from the front of the dwelling.
- A high level of articulation and architectural interest in the built form, such as the use of balconies, shading devices, clear front entry ways, mix of textures/ materials and window treatments, is required along the front of the dwelling, to drainage reserves and to open space frontages so as to facilitate a quality development as viewed from all surrounds.

Fencing

• Fences to Road Reserve/ Drainage shall be no higher than 1.5 metres and shall be constructed of a tubular (steel or aluminium) type fencing and black in colour. No solid features or appendages shall be added to these fences due to wind loading requirements.

Private Open Space

- There is no minimum open space requirement applicable to these lots if the setbacks and courtyard requirements are achieved, otherwise the provisions of the R-Codes apply.
- An alternative courtyard location may be approved for corner lots or those abutting a laneway provided appropriate solar protection is provided to prevent excessive heat gain to the interior of the dwelling and the courtyard and the cross ventilation requirements of the Madigan Road Design Guidelines are adequately demonstrated.

Servicing

- Air conditioners or cooling units must not be easily visible from the street / drainage reserve and be positioned to minimise noise impacts on neighbouring residences.
- Tanks associated with solar hot water units must either be ground mounted or match the roof profile and pitch and, where possible, not be easily seen from the street / reserve.
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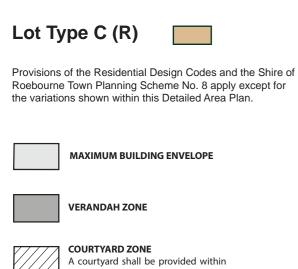
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LOCAL DEVELOPMENT PLAN

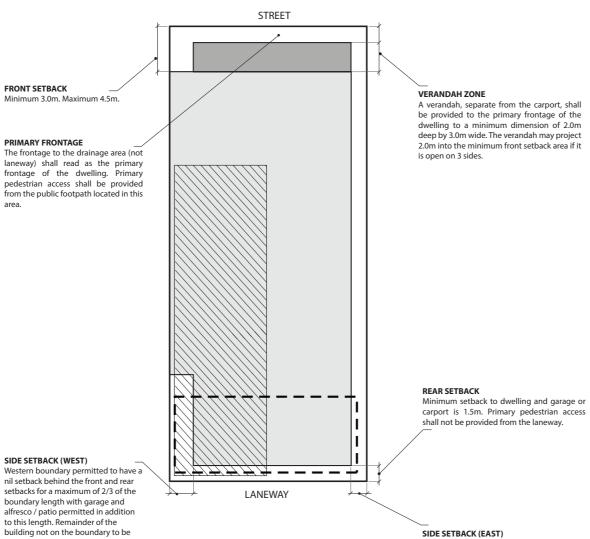
+61 08 9289 8300 www.elementwa.com.au



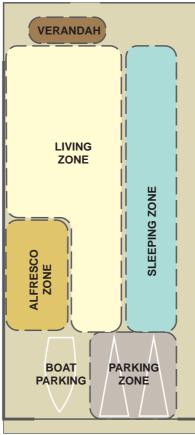
A courtyard shall be provided within this zone with a minimum area of 20m2, a minimum covered space of 15m2 and a minimum dimension of 4.0m.

CARPORT/ GARAGE ZONE

An open carport or enclosed garage is to be provided within this zone and shall be able to accommodate two



Street



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Context Plan - Lot Type C (R)

Provisions

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setback a minimum of 1.5m.

Building Envelope

- Height as per Category B standards of R-Codes (2 storey).
- Dwellings shall be a minimum of 2 storeys.
- · Overshadowing No restrictions

Garage/ Outbuildings

- Freestanding sheds and storerooms must be constructed of the same or complementary materials, finishes, colours and roof style types as the main house to the satisfaction of the Shire of Roebourne and not be located within the front setback.
- Garage / carport may have a nil setback to the side boundary where appropriately fire rated.
- No back gates are permitted where they encroach into the laneway when opening.

Building Frontage

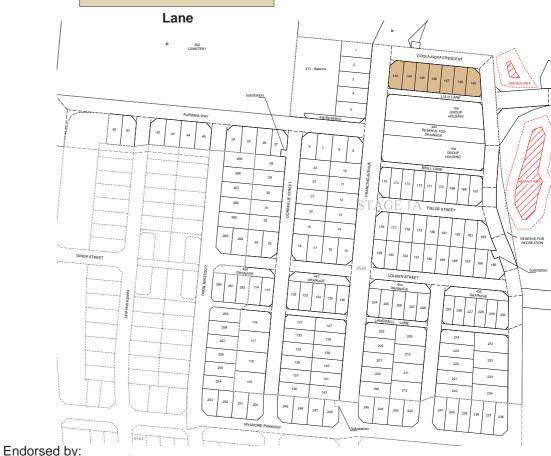
- Dwelling to address the primary street by way of design, fenestration, entry and must contain major opening(s).
- Front doors of dwellings shall be clearly visible and recognisable from the front of the dwelling.
- A high level of articulation and architectural interest in the built form, such as the use of balconies, shading devices, clear front entry ways, mix of textures/ materials and window treatments, is required along all public roads, to drainage reserves and to open space frontages so as to facilitate a quality development as viewed from all surrounds.

Private Open Space

- There is no minimum open space requirement applicable to these lots if the setbacks and courtyard requirements are achieved, otherwise the provisions of the R-Codes apply.
- An alternative courtyard location may be approved for corner lots or those abutting a laneway provided appropriate solar protection is provided to prevent excessive heat gain to the interior of the dwelling and the courtyard and the cross ventilation requirements of the Madigan Road Design Guidelines are adequately demonstrated.

Servicing

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Level 18, 191 St Georges Terrace Perth Western Australia 6000

Date: 16 Apr 2020

21/4/2020

Date

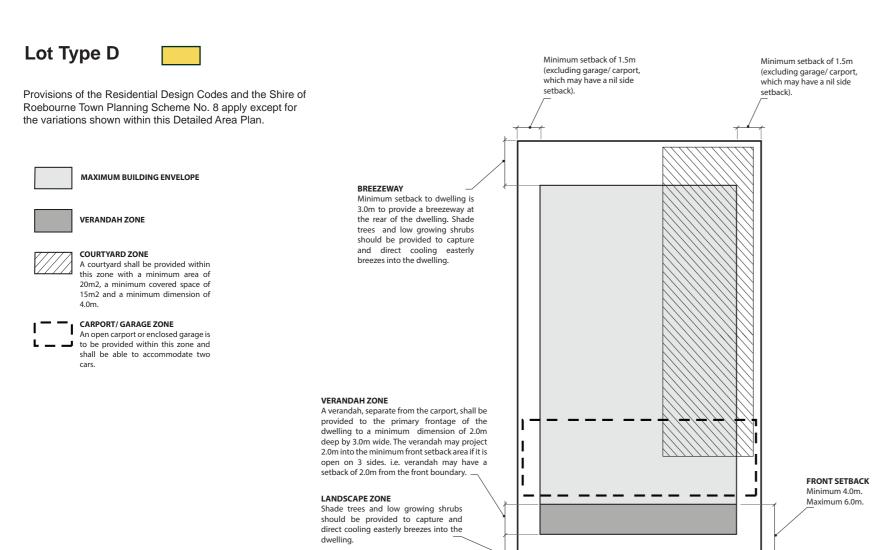
City of Karratha

Karratha, Madigan Road

Drawing No. 710-200A Karratha, Madigan Road.indd

LOCAL DEVELOPMENT PLAN

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Garage/ Outbuildings

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- · Carports and garages shall be set back 4.5m from the front boundary to allow for additional car parking within the front setback.

Building Frontage

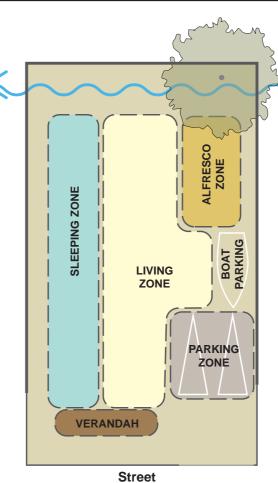
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Servicina

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Indicative Floor Plan

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Context Plan - Lot Type D

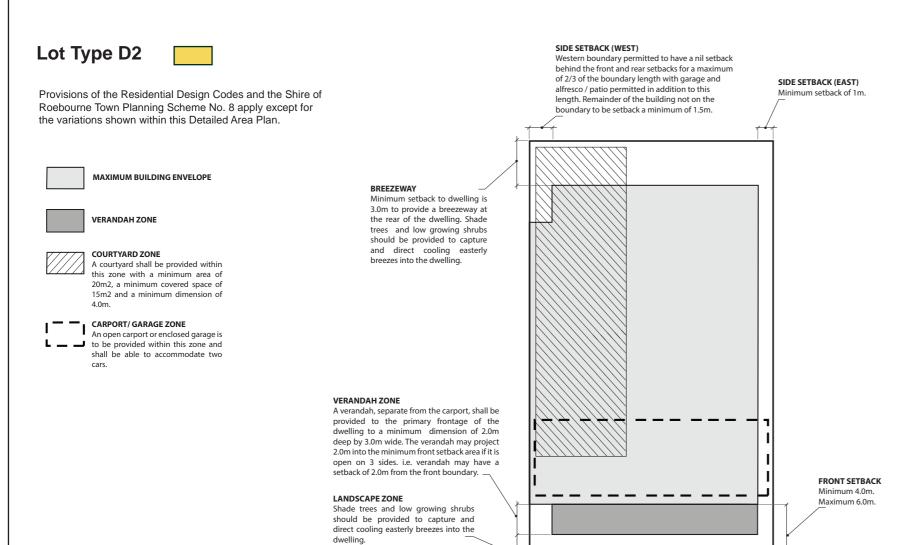


Endorsed by: 21/4/2020 City of Karratha

Karratha, Madigan Road LOCAL DEVELOPMENT PLAN

Date: 16 Apr 2020





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Garage/ Outbuildings

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Building Frontage

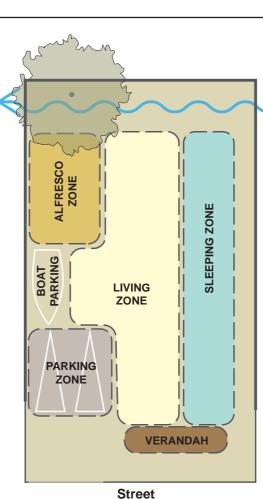
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Indicative Floor Plan

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Context Plan - Lot Type D2

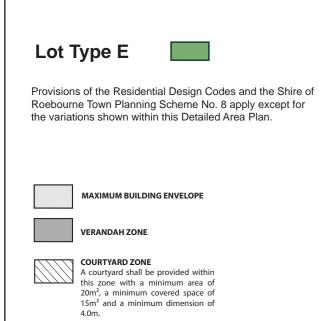


City of Karratha

Karratha, Madigan Road LOCAL DEVELOPMENT PLAN Date: 16 Apr 2020 21/4/2020

Drawing No. 710-200A Karratha, Madigan Road.indd





STREET LANDSCAPE ZONE Shade trees and low growing shrubs should be provided to capture and direct FRONT SETBACK cooling easterly breezes into the dwelling. Minimum 4.0m. Maximum 6.0m VERANDAH ZONE A verandah shall be provided to the primary frontage of the dwelling to a minimum dimension of 2.0m deep by 3.0m wide. The verandah may project 2.0m into the minimum front setback area if it is open on 3 sides. i.e. verandah may have a setback of **BREEZEWAY** Minimum setback to dwelling is 3.0m to provide a breezeway at the rear of the dwelling. Shade trees and low growing shrubs should be provided to capture and direct cooling easterly breezes into the dwelling

SIDE SETBACK (WEST) Minimum setback of 1.5m

(excluding garage/ carport,

which may havd a nil side

sethack

Street **Indicative Floor Plan**

VERANDAH

ZONE

SLEEPING

LIVING

ZONE

PARKING

ZONE

BOAT PARKING

City of Karratha

SIDE SETBACK (EAST)

(excluding carport/ garage,

which may have a nil side

setback).

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Context Plan - Lot Type E

Provisions

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Building Envelope

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- · Overshadowing No restrictions

T ENCLOSED GARAGE ZONE

An open carport or enclosed garage is to be provided within this zone and shall be able to accommodate two

cars. An enclosed carport shall be set

back 0.5 metres behind the front

building line (Front building line:

average front setback excluding the

Garage/ Outbuildings

- Maximum garage width is 7.0m (double door) including supporting structures.
- Freestanding sheds and storerooms must be constructed of the same or complementary materials, finishes, colours and roof style types as the main house to the satisfaction of the Shire of Roebourne and not be located within the front setback.
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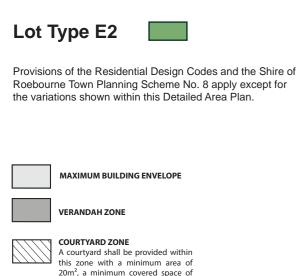


Date: 16 Apr 2020

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Date

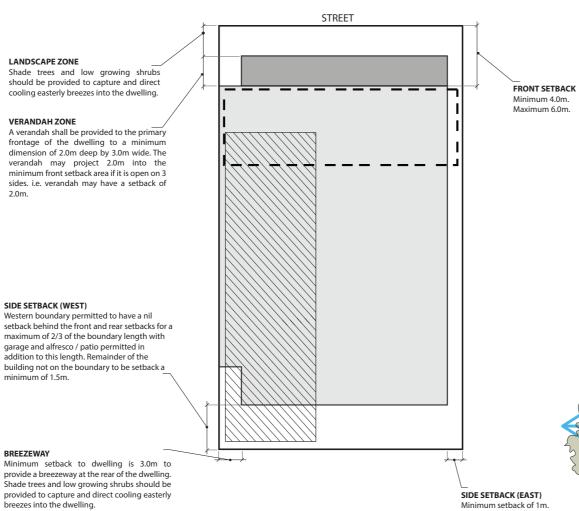
LOCAL DEVELOPMENT PLAN



15m² and a minimum dimension of

PINCLOSED GARAGE ZONE

An open carport or enclosed garage is to be provided within this zone and shall be able to accommodate two cars. An enclosed carport shall be set back 0.5 metres behind the front building line (Front building line: average front setback excluding the



Indicative Floor Plan

Street

LIVING

ZONE

PARKING

ZONE

BOAT PARKING **VERANDAH**

ZONE

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Lot Type F SIDE SETBACK (EAST) SIDE SETBACK (WEST) Provisions of the Residential Design Codes and the Shire of Western boundary permitted to have a nil setback behind the front and rear setbacks Roebourne Town Planning Scheme No. 8 apply except for LANEWAY the variations shown within this Detailed Area Plan. for a maximum of 2/3 of the boundary length with garage and alfresco / patio permitted in addition to this length. Remainder of the MAXIMUM BUILDING ENVELOPE building not on the boundary to be setback a Excluding eaves and other minor encroachments such as chimneys, REAR SETBACK awnings etc. Minimum setback to dwelling is 1.0m. An enclosed garage or open carport may be set VERANDAH ZONE back at nil to the laneway boundary. COURTYARD ZONE A courtyard shall be provided within this zone with a minimum area of 20m2, a minimum covered space of 15m2 and a minimum dimension of

■ ■ CARPORT/ GARAGE ZONE

An open carport or enclosed garage is to be provided within this zone and shall be able to accommodate two cars. Any garage door to a carport / garage shall be set back 2.5 metres behind the front verandah posts /

VERANDAH ZONE

A verandah shall be provided to the primary frontage of the dwelling to a minimum dimension of 2.0m deep by 3.0m wide. The verandah may project 2.0m into the minimum front setback area if it is open on 3 sides i.e. verandah may have a setback of 1.0m from the front boundary.

PRIMARY FRONTAGE

The frontage to the Primary Street (not laneway) shall read as the primary frontage of the dwelling. Primary pedestrian access shall be from this frontage.

City of Karratha

FRONT SETBACK

Maximum 4 5m

STREET

Provisions

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Garage/ Outbuildings

- Freestanding sheds and storerooms must be constructed of the same or complementary materials, finishes, colours and roof style types as the main house to the satisfaction of the Shire of Roebourne and not be located within the front setback.
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- Dwelling to address the primary street by way of design, fenestration, entry and must contain major opening(s).
- · Front doors of dwellings shall be clearly visible and recognisable from the front of the dwelling.
- · A high level of articulation and architectural interest in the built form, such as the use of balconies, shading devices, clear front entryways, mix of textures/ materials and window treatments, is required to the front facade.

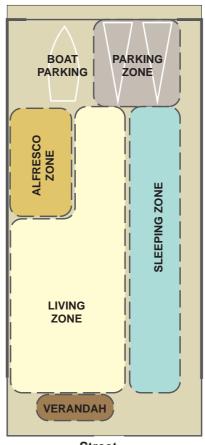
Private Open Space

- There is no minimum open space requirement applicable to these lots if the setbacks and courtyard requirements are achieved, otherwise the provisions of the R-Codes apply.
- An alternative courtyard location may be approved for corner lots or those abutting a laneway provided appropriate solar protection is provided to prevent excessive heat gain to the interior of the dwelling and the courtyard and the cross ventilation requirements of the Madigan Road Design Guidelines are adequately demonstrated.

Servicing

- · Air conditioners or cooling units must not be easily visible from the street and be positioned to minimise noise impacts on neighbouring residences.
- Tanks associated with solar hot water units must either be ground mounted or match the roof profile and pitch and, where possible, not be easily seen from the street.
- Minor variations to the requirements of this Detailed Area Plan may be approved by the Shire of Roebourne without the need to obtain a separate planning approval. Significant variations will require a planning approval pursuant to the Scheme.

Lane



Indicative Floor Plan

This plan provides an example indicative floor plan to assist with interpreting the intent and principles outlined within the Detailed Area Plan. It does not form part of this Detailed Area Plan.

Context Plan - Lot Type F



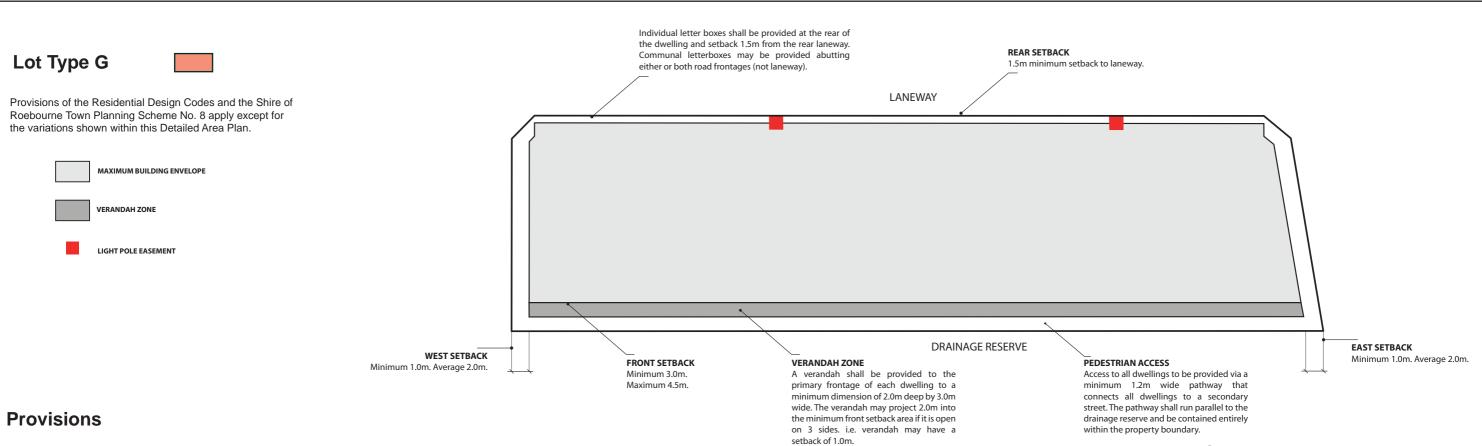
Date: 16 Apr 2020

21/4/2020

Date

Karratha, Madigan Road

LOCAL DEVELOPMENT PLAN



General Design Intent

• The design intent is outlined by the Madigan Estate Design Guidelines and Karratha Vernacular Design Principles. All development must give due regard to the requirements and intent of these documents.

Building Envelope

- Height as per Category B standards of R-Codes (2 storey).
- An upper level zero lot line parapet wall may only be permitted if abutting a parapet wall of the same height and dimensions of the neighbouring dwelling. Otherwise an upper level setback of 1.5m applies, measured from the relevant outer ground floor wall below.
- Unless constructing a studio apartment above a garage, any second storey shall be constructed on the southern portion of the lot to address the drainage reserve.
- A verandah may project a maximum of 2.0m into the front setback area if it is open on 3 sides.
- · Overshadowing No restrictions.

Pedestrian and Vehicle Access

- · Vehicle access shall be provided from the laneway frontage.
- Primary pedestrian access shall be provided from the drainage frontage via a minimum 1.2m wide pathway that connects all dwellings to a secondary street. The pathway shall run parallel to the drainage reserve and be contained entirely within the property boundary.
- Front doors of dwellings shall be located to address the drainage reserve frontage and be clearly visible and recognisable.

• Freestanding sheds and storerooms must be constructed of the same or complementary materials, finishes, colours and roof style types as the main house to the satisfaction of the Shire of Roebourne and not be located within the front setback.

- The drainage reserve is the primary frontage of the lot. Dwellings to address the drainage reserve by way of design, fenestration, entry and must contain major opening(s).
- A high level of articulation and architectural interest in the built form, such as the use of balconies, shading devices, clear front entry ways, mix of textures/ materials and window treatments, is required to the front facade.

Fencing

- Fences to Road Reserve/ Drainage shall be no higher than 1.5 metres and shall be constructed of a tubular (steel or aluminium) type fencing and black in colour. No solid features or appendages shall be added to these fences due to wind loading requirements.
- · Alteration to any existing retaining wall is not permitted.
- No back gates are permitted where they encroach into the laneway when opening.

- An external courtyard with a minimum area of 20m² and a minimum dimension of 3 metres shall be provided to abut the southern and/or eastern side of each ground floor dwelling. The courtyard shall be directly accessed via an internal living area (excluding bedroom).
- Private open space for upper level apartments shall be provided in accordance with the R-Codes.
- There is no minimum open space requirement applicable to these lots if the setbacks and courtyard requirements are achieved, otherwise the provisions of the R-Codes apply.

- Air conditioners or cooling units must not be easily visible from the street/ drainage reserve and positioned to minimise noise impacts on neighbouring residences.
- Tanks associated with solar hot water units must either be ground mounted or match the roof profile and pitch and, where possible, not be easily seen from the street.
- Minor variations to the requirements of this Detailed Area Plan may be approved by the Shire of Roebourne without the need to obtain a separate planning approval. Significant variations will require a planning approval pursuant to the Scheme.



Endorsed by:

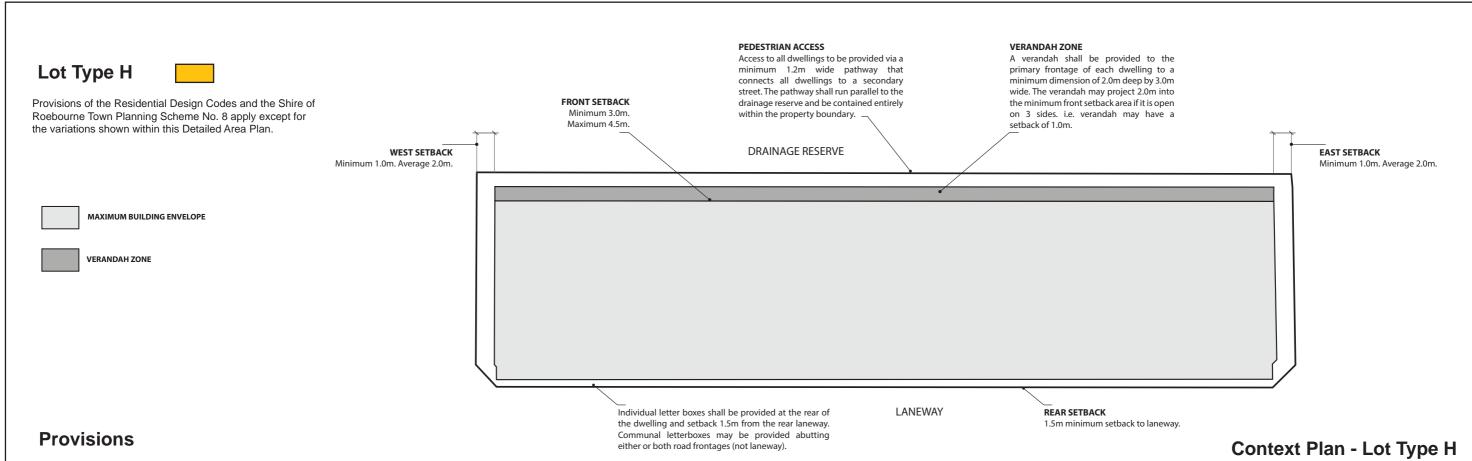
City of Karratha

21/4/2020 Date

Karratha, Madigan Road LOCAL DEVELOPMENT PLAN

Date: 16 Apr 2020 Drawing No. 710-200A Karratha, Madigan Road.indd





General Design Intent

• The design intent is outlined by the Madigan Estate Design Guidelines and Karratha Vernacular Design Principles. All development must give due regard to the requirements and intent of these documents

Building Envelope

- Height as per Category B standards of R-Codes (2 storey).
- An upper level zero lot line parapet wall may only be permitted if abutting a parapet wall of the same height and dimensions of the neighbouring dwelling. Otherwise an upper level setback of 1.5m applies, measured from the relevant outer ground floor wall below.
- Unless constructing a studio apartment above a garage, any second storey shall be constructed on the northern portion of the lot to address the drainage reserve.
- A verandah may project a maximum of 2.0m into the front setback area if it is open on 3 sides.
- · Overshadowing No restrictions.

Pedestrian and Vehicle Access

- Vehicle access shall be provided from the laneway frontage.
- Primary pedestrian access shall be provided from the drainage frontage via a minimum 1.2m wide pathway that connects all dwellings to a secondary street. The pathway shall run parallel to the drainage reserve and be contained entirely within the property boundary.
- Front doors of dwellings shall be located to address the drainage reserve frontage and be clearly visible and recognisable.

Outbuildings

• Freestanding sheds and storerooms must be constructed of the same or complementary materials, finishes, colours and roof style types as the main house to the satisfaction of the Shire of Roebourne and not be located within the front setback.

Building Frontage

- The drainage reserve is the primary frontage of the lot. Dwellings to address the drainage reserve by way of design, fenestration, entry and must contain major opening(s).
- · A high level of articulation and architectural interest in the built form, such as the use of balconies, shading devices, clear front entry ways, mix of textures/ materials and window treatments, is required to the front facade.

Fencing

- Fences to Road Reserve/ Drainage shall be no higher than 1.5 metres and shall be constructed of a tubular (steel or aluminium) type fencing and black in colour.
- Alteration to any existing retaining wall is not permitted.
- No back gates are permitted where they encroach into the laneway when opening.

Private Open Space

- An external courtyard with a minimum area of 20m² and a minimum dimension of 3 metres shall be provided to abut the southern and/or eastern side of the dwelling. The courtyard shall be directly accessed via an internal living area (excluding bedroom).
- Private open space for upper level apartments shall be provided in accordance with the R-Codes.
- There is no minimum open space requirement applicable to these lots if the setbacks and courtyard requirements are achieved, otherwise the provisions of the R-Codes apply.

Servicing

- Air conditioners or cooling units must not be easily visible from the street/ drainage reserve and positioned to minimise noise impacts on neighbouring residences.
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Endorsed by:

City of Karratha



21/4/2020

Date

Karratha, Madigan Road LOCAL DEVELOPMENT PLAN

Date: 16 Apr 2020